The background of the slide is a blue-tinted architectural rendering of a residential development. It features a mix of housing styles, including multi-story apartment buildings and smaller, single-story houses. A semi-transparent wireframe grid is overlaid on the entire scene, creating a technical, architectural feel. The text is centered and rendered in a bold, dark blue, sans-serif font.

WEST TEMPLE GATEWAY PROJECT AREA DESIGN GUIDELINES

2011 03 25

WTG DESIGN GUIDELINES

A. Design Intent

1. Design Vision
2. Purpose of this Document
Key Design Objectives

B. Street Typologies

Street Classification Map

1. Type I (Transit Boulevard)
 - a. Descriptions
 - b. Standards
 - c. Diagram
2. Type II (Fleet Multi-Modal Street)
 - a. Descriptions
 - b. Standards
 - c. Diagram
3. Type III (Residential Street)
 - a. Description
 - b. Standards
 - c. Diagram

C. Site Design

1. Building Placement
2. Parking Lot Screening and Landscaping
3. Lighting
4. Pedestrian Connections
5. LID/Sustainable Features
6. Corner Sites
7. Treatment of outdoor storage, sales, and equipment

D. Building Design

1. Ground floor details
2. Ground level transparency
3. Prominent Entrances
4. Treatment of Blank Walls
5. Articulation
6. Transition of Scale

E. Sign Design

1. Integration with Architecture
2. Creativity and Unique Expression
3. Pedestrian Orientation
4. Wayfinding Coordination

A. DESIGN INTENT

1. Design Vision

The West Temple Gateway Project Area is located along the Trax line southwest of downtown Salt Lake City. The scale of development ranges from large scale multifamily and commercial uses to single family housing on less than 5000 SF lots. The vision for new development in this area is to create a walkable, transit-oriented neighborhood by fostering the continued pattern of small scale infill projects as well as allowing for context-sensitive larger scale mixed-use projects that will provide a wide range of housing choices and incubator commercial spaces that serve the neighborhood and the region.

2. Purpose of this Document

The purpose of this document is to serve as a design guide for redevelopment in the West Temple Gateway Project Area. The guidelines in this document are directly related to achieving the key design objectives for the district:

Key Design Objectives

- A. Context-sensitive solutions for infill and redevelopment projects in the project area.
- B. Emphasis on mixed-use, pedestrian-oriented developments and streetscapes that promote active use of the streets, sidewalks and public spaces.
- C. Incorporate the CPTED principles (Crime Prevention Through Environmental Design) to increase “eyes on the street” and other considerations in regards to public safety.
- D. Ensure availability of a range of transportation choices including all modes – walking, bicycling, transit, and motor vehicles.
- F. Apply principles of long term economic, social, and environmental sustainability in the design of infrastructure, site and building development.
- G. Provide the West Temple Gateway Project Area with a distinct character.

Each guideline includes an intent statement that explains the purpose of the directive to achieve one or more of these overall design objectives. In many cases alternative solutions to the guidelines may be suggested by the developer/designer/applicant, so long as the solution meets the intent statement.

B. DESIGN GUIDELINES: STREET TYPOLOGY

1. Type I Streets (Transit Boulevard)

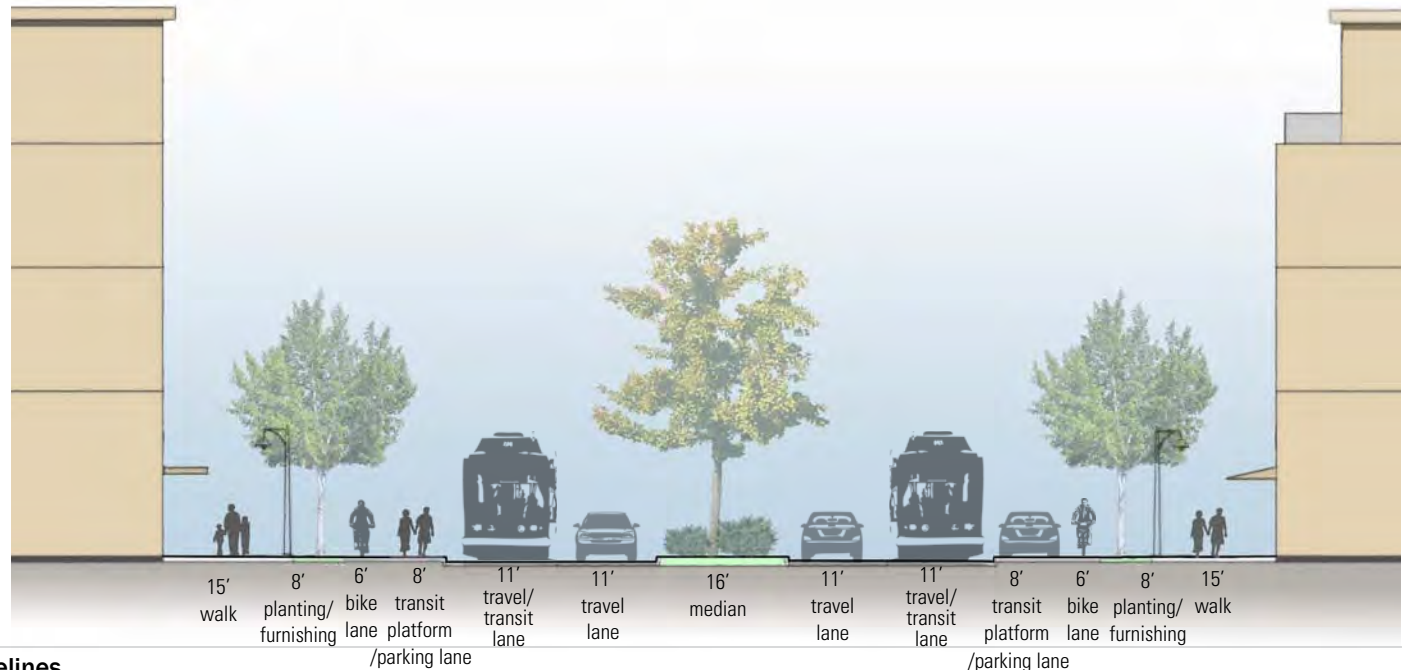
a. Description

These streets or street segments shall primarily support pedestrian and transit circulation while also fully accommodating vehicles. Some traffic calming features may be used and speed limits shall be 30 mph or less.

b. Street Standards

- i. The pedestrian realm should include an 8-foot (5-foot minimum) paved furnishings zone behind the curb for plantings, poles and standards, and other furnishings; and a 15-foot (9-foot minimum) clear passageway for walking.
- ii. To ensure smoother, more organized traffic movements and less disruption of pedestrian movement, curb cuts (driveway openings) should:
 - a) Be no more than 30 feet wide.
 - b) A minimum of 200 feet apart from any other curb cut or street intersection, except as required to provide minimum access to a property
 - c) Not interrupt the paving material of the sidewalk with another material. The sidewalk paving should be continuous.
- iii. Curb extensions at intersections are required except at streetcar corners if a streetcar line is installed in the future. Turn radii (curb returns) shall be 15 feet or less on all multi-lane streets.
- iv. Special lighting and furnishings (such as benches, light fixtures, news stands, garbage/recycling receptacle, etc) are required to be located within the furnishings zone.
- v. Trees should be planted within the furnishings zone at the average rate of one tree every 25 feet of street frontage. Trees may be spaced at irregular intervals to accommodate sight distance requirements for driveways and intersections.
- vi. A designated area for bicycle traffic should be accommodated within the street ROW for both directions of travel, this could be accomplished by a 6-foot bicycle lane on both sides of the street between the on-street parking and the travel lanes, a separated bike path between the planting and parking lane, two bike lanes within a widened traffic median, or other similar solution.
- vii. Where left turn movements need to be specifically accommodated, planted medians with turn pockets should be installed wherever possible rather than a continuous center turn lane.

c. Street Section Diagram



B. DESIGN GUIDELINES: STREET TYPOLOGY

2. Type II Streets (Fleet Multi-Modal Streets)

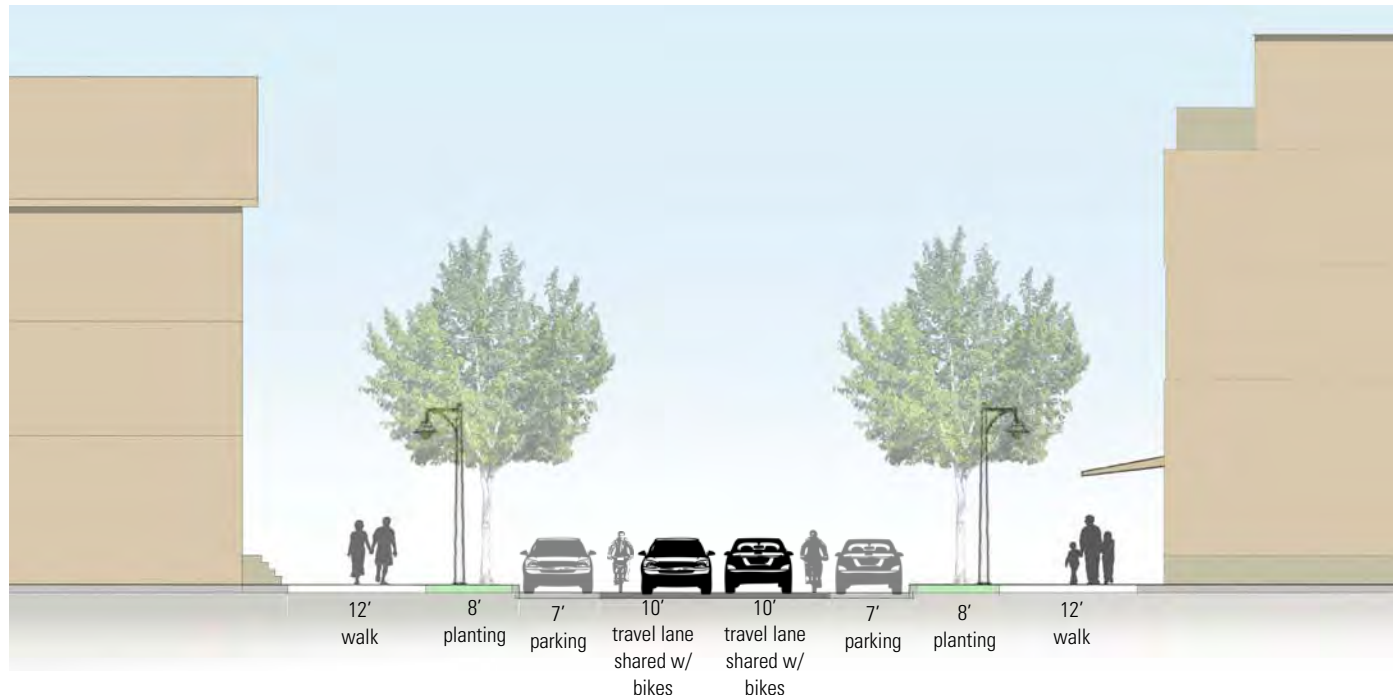
a. Description

These streets are intended to function as the “pedestrian first street” in terms of the building-street relationship and the high degree to which pedestrian comfort and safety are addressed through streetscape design and minimization of vehicle-pedestrian conflict points. Although vehicular traffic is not excluded, its movement shall be slow (30 mph or lower) and “calmed” through features such as curb extensions, on-street parking, and frequent crossings. Sidewalks should be wide, street trees and understory vegetation should be plentiful (in planters), incorporating pedestrian-scaled lighting and abundant street furnishings.

b. Street Standards

- i. The pedestrian realm should include an 8-foot (5-foot minimum) paved furnishings zone behind the curb for plantings, poles and standards, and other furnishings; and a 12-foot (9-foot minimum) clear passageway for walking.
- ii. To ensure smoother, more organized traffic movements and less disruption of pedestrian movement, curb cuts shall:
 - a) Be no more than 30 feet wide.
 - b) Be spaced a minimum of 200 feet apart from another curb cut or street intersection, except as required to provide minimum access to a property.
 - c) Not interrupt the paving material of the sidewalk with another material. The sidewalk paving shall be continuous.
- iii. Curb extensions at intersections should be installed wherever possible.
- iv. Sidewalk lighting and furnishings (such as benches, light fixtures, news stands, garbage/recycling receptacle, etc) shall be placed within the furnishings zone behind the curb.
- v. Bicycle traffic should be accommodated in the travel lane using a method such as sharrows*, or other similar solution.
- vi. Parallel on-street parking should be installed on both sides of the street wherever possible.

c. Street Section Diagram



**This study acknowledges that although SLC Transportation recommended that a separate bike lane be added to the streets in these guidelines, due to the scale of Street type 2 and the desire to slow traffic for a more ideal pedestrian environment within the Fleet Block, sharrows are recommended here.*

3. Type III Streets (Residential Streets)

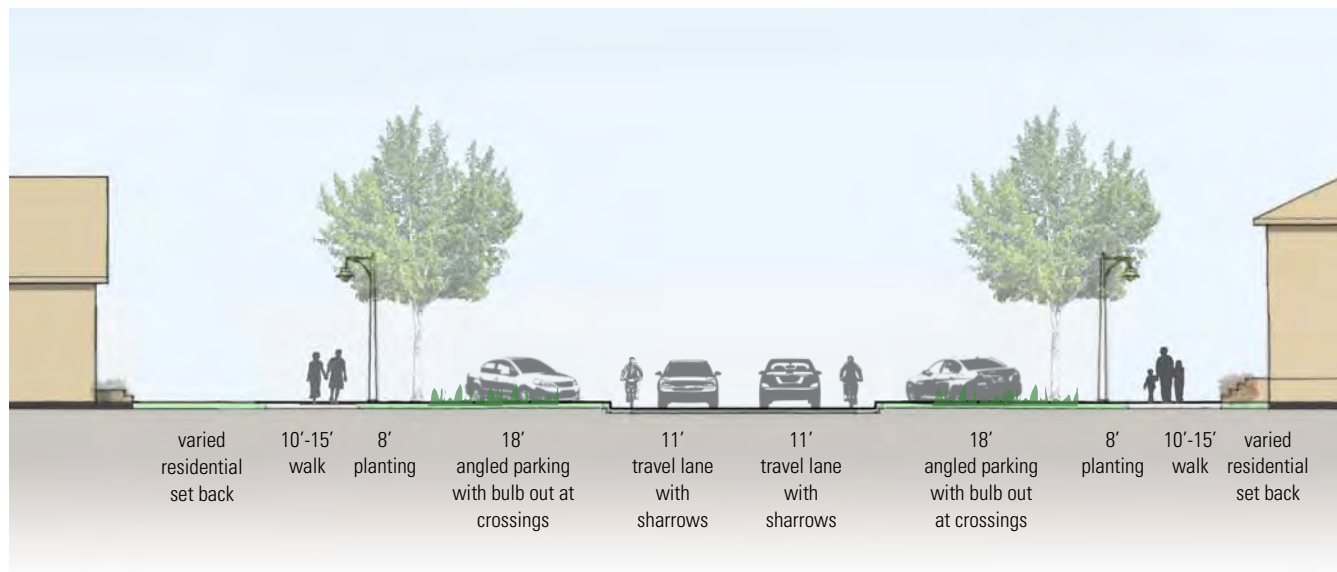
a. Description

This street type should be designed to emphasize walking, biking, and residential access over motor vehicle circulation. Generous provision of plantings and street trees, and on-street parking shall contribute to a pedestrian-oriented environment. Vehicular movement shall be 25 mph and “calmed” through features such as curb bulbs, on-street parking, and frequent crossings.

b. Street Standards

- i. The pedestrian realm shall include an 8-foot (5-foot minimum) paved furnishings zone behind the curb for plantings, poles and standards, and other furnishings; and a 15-foot (9-foot minimum) clear passageway for walking.
- ii. To ensure smoother, more organized traffic movements and less disruption of pedestrian movement, curb cuts should:
 - a) Be no more than 30 feet wide.
 - b) Be spaced a minimum of 200 feet apart from another curb cut or street intersection, except as required to provide minimum access to a property.
 - c) Not interrupt the paving material of the sidewalk with another material. The sidewalk paving should be continuous.
- iii. Curb extensions at intersections shall be installed wherever possible.
- iv. Sidewalk lighting is required within the furnishings zone
- v. Angled, back-in on-street parking shall be installed wherever possible on both sides of the street.
- vi. Bicycle traffic should be accommodated in the travel lane using a method such as sharrows.
- vii. LID (low impact development) features, gardens, and public art are encouraged in the designated planting areas

c. Street Section Diagram



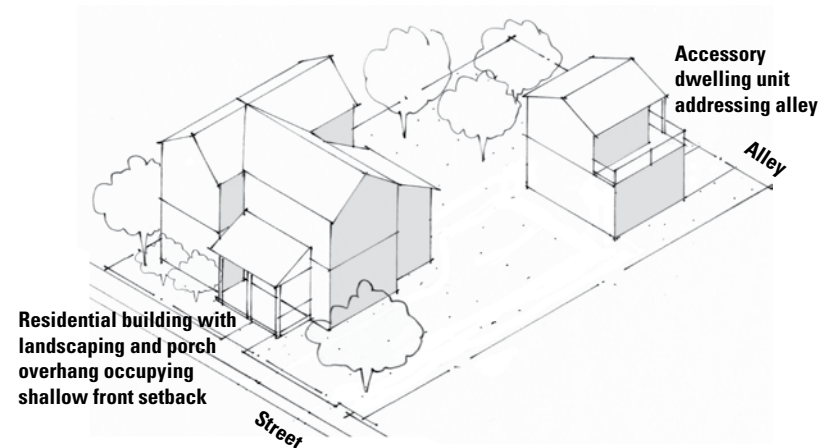
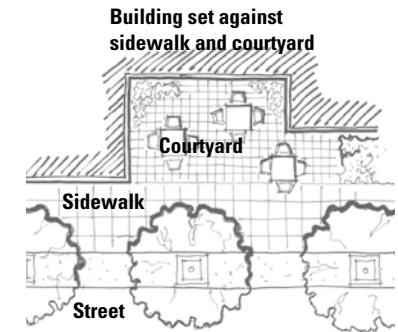
1. Building Placement

Intent: To support and encourage pedestrian comfort, convenience and activity by creating a sense of enclosure within the street corridor, by establishing a direct relationship between buildings and sidewalks, and by creating a logical transition from the public realm of the streets and sidewalks to the private realm of residential properties.

1. Commercial and mixed use buildings should be built along or within 18 inches of the back of the sidewalk on all Type I and Type II streets, adjacent to any public plaza, courtyard, seating area, or other space intended for public use.
2. Multi-family buildings may include a modest front setback (3-7 feet) to create a transition area between the public and private space. Street wall reinforcing elements are encouraged to occupy in this setback, such as:
 - Porches/stoops
 - Landscaping
 - 3' maximum fence
3. Single-family and lower density residential structures on Type III streets may have a front setback of 20-25', (or average of two adjacent properties) to maintain the existing character.
4. Detached accessory residential structures, such as accessory dwelling units or detached garages should be set 0'-10' from the back lot line and orient towards the alley.



Buildings sited at the back of sidewalk to make a strong street wall.



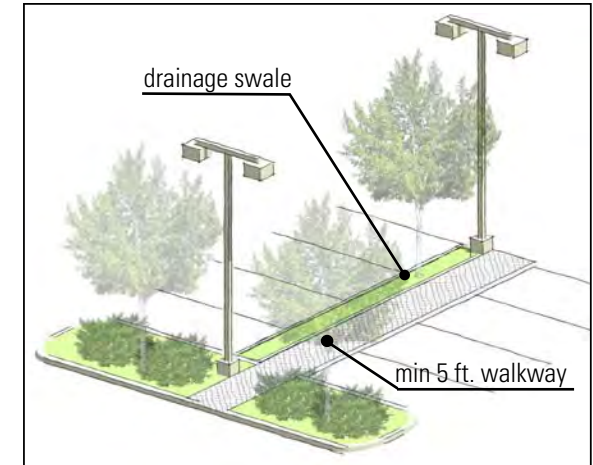
2. Parking Lot Screening and Landscaping

Intent: To diminish the amount of impervious surface and visual impact of parked cars and to buffer parking lots from other uses of lesser intensity to provide for infiltration of rain water and snow melt, to offer shade to otherwise bare paved areas, and to visually soften expanses of parking.

1. Parking lots should integrate main drive aisles to appear more like streets, and should include sidewalks, landscaping including trees, and pedestrian scaled lighting.
2. Masonry walls and other structural screening features should be used only for corner accents or where screening of headlights is necessary, and should not be used as a substitute for landscaping.
3. Parking aisle should be organized to create a central pedestrian access to building entries. Outer parking aisles may incorporate drainage swales between parking rows.
4. Trees should be distributed throughout the parking area to provide ample shading and visually soften the parking area, roughly 1 tree for every 8 parking stalls. Adjacent to single-family residential uses, 1 tree for every 5 stalls should be planted.
5. In addition to trees, shrubs and perennials should be planted as understory at the base of tree planting beds.
6. Grouping trees may be allowed to accommodate natural features, so long as the equivalent number of trees are planted and so long as the grouping is within the parking area. Curbs or other methods of preventing vehicles from damaging the trees should be installed.
7. Retaining existing trees in parking lots is encouraged.



Planting strip between sidewalk and parking lot.



Walkway through parking lot with landscaped drainage.

3. Lighting

Intent: To ensure that lighting contributes to the character and safety of the streetscape and public spaces, but does not disturb adjacent developments and residences.

1. Use City-approved standardized fixtures for sidewalk lighting. Fixtures should be consistent with adopted light fixture for the project area.
2. Lighting elements throughout and surrounding the site should be complementary, including pedestrian pathway, accent, and parking lot lighting, lighting of adjacent developments and the public right-of-way.
3. All lighting should be shielded from the sky and adjacent properties and structures, either through exterior full cut-off shields or through optics within the fixture.
4. Lighting used in parking lots should not exceed a maximum of 30 feet in height. Pedestrian-scale lighting should be a maximum of 16 feet in height.
5. Parking lot lighting should be appropriate to create adequate visibility at night and evenly distributed to increase security.



Examples of cut-off lighting fixtures.

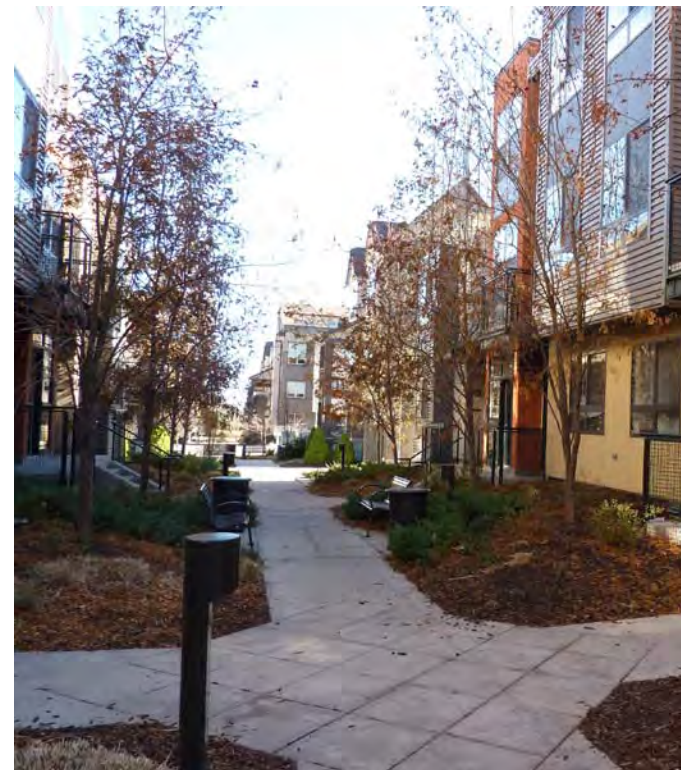
4. Pedestrian Connections

Intent: To allow for safe pedestrian passage through any large blocks to provide convenient and direct pedestrian connections throughout the district and to provide neighborhood-scale open space.

1. Formalized mid-block pedestrian corridors or connections between public rights of way through the blocks and redevelopment sites on 300'-350' intervals are highly encouraged, with at least one through-block connection for any block face longer than 600'.
2. All non-motorized corridors and connections should include:
 - a 5' minimum building setback on either side of the connection, which could include landscaping, lighting, and other pedestrian amenities,
 - a 5' minimum walkway, and
 - appropriately scaled pedestrian lighting
 - CPTED considerations in regards to landscaping and other potential hazardous areas.
3. Walkways should be paved with a differentiated pavement surface treatment to alert drivers to the pedestrian right of way and potential presence of pedestrians. Speed tables may be installed as appropriate to further calm vehicular traffic.
4. Alternate building entrances are encouraged to be located on pedestrian connections and alley ways to provide a building face along such pathways.
5. Access from the street should include wayfinding signage to notify pedestrians of the facility. (see Sign Design on page 20)



Pedestrian connection with lighting and landscaping.



Pedestrian connection with entries and windows along walkway.

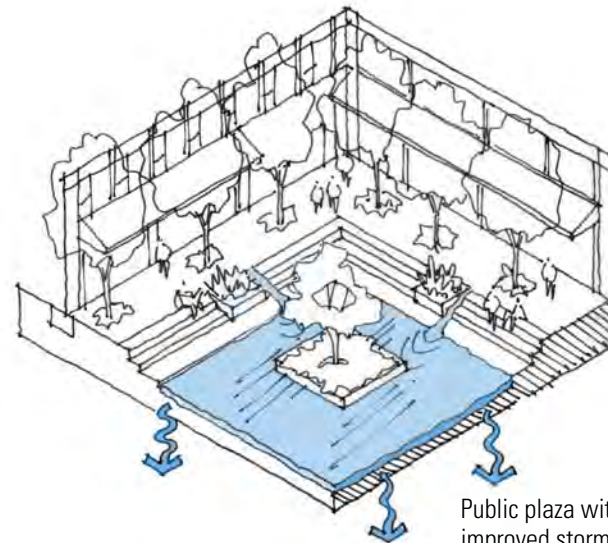
5. LID/Sustainable Features

Intent: To encourage the integration of the natural systems, such as weather and water, into site design.

1. Site design and building placement should address specific natural conditions such as: significant wind or sun exposure, views or other natural features to ensure the development is designed in keeping with the context of its environment.
2. The placement of any structure on the site should consider solar access to minimize shadow impacts on adjacent structures and public areas.
3. The following Low Impact Development (LID) techniques are encouraged for site planning to ensure that sensitive areas are protected and maintained:
 - a. Minimize impervious surface for all development wherever feasible.
 - b. Implement stormwater retention techniques that capture stormwater close to where it falls to mimic natural systems wherever possible. Examples include: bioretention swales and ponds, stormwater capture (per Utah State Law SB 32), and vegetated roofs.
 - c. Other techniques that satisfy the intent of low impact development, such as:
 - xeriscaping or low-water landscaping,
 - retain existing vegetation on site,
 - use of alternative energies (ie solar/wind power) for utilities,
 - use paving materials that reflect as opposed to absorbing solar heat,
 - etc.



Streetscape with lid stormwater features integrated into landscaping design.



Public plaza with pervious paving for improved stormwater management.

6. Corner Sites

Intent: To provide an enhanced pedestrian experience by creating visual gateways, public plazas, courtyards and other gathering spaces.

1. Key intersections should be marked with setbacks that allow for public spaces. Rather than meeting the corner, new buildings should incorporate forecourts, plazas, or gardens that welcome the public and offer a dramatic statement at the corner.
2. Major entrances should also be located at the corners and highlighted by elements such as higher or more expressive canopies, higher bays, larger windows and doors, projections, different window designs, or other physical features.
3. If potential views to noteworthy natural features and points of interest exist, (either nearby or in the distance exist from the development site), entrances and publicly accessible open spaces should be located and oriented to take advantage of this view.

KEY INTERSECTIONS



Courtyard plaza at street corner, framed by building oriented to public space.



Principal building entrance from sidewalk with a gracious entry court.

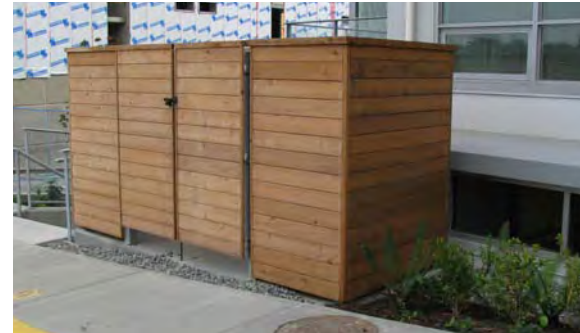
7. Treatment of Outdoor Storage, Sales, and Equipment

Intent: To reduce the visual impacts of storage, trash, and service areas.

1. The total area allowed for outdoor storage and/or merchandise display should be less than twenty-five percent (25%) of the total gross square footage of building occupied by the use; provided, however, that such area may exceed twenty-five (25%) percent if it is fenced and screened. This standard does not apply to temporary uses such as material storage during construction or street vendors.
2. Any storage, service and truck loading areas, utility structures, storage tanks, elevator and mechanical equipment on the ground or roof should be screened from public view.
3. Trash collection and outdoor storage tank areas should be located within enclosed structures constructed of similar materials and quality of the associated buildings, with a gate that can be closed. The gate should be similarly treated or located in an area not visible from the street.



Architectural screening of rooftop mechanical equipment.

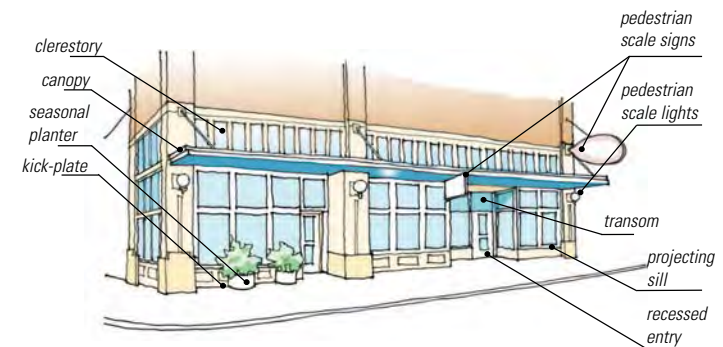


Trash and/or storage enclosure.

1. Ground Floor Details

Intent: To reinforce the character of the streetscape and provide pedestrian amenities.

1. The first floor level should be at least 12 feet in height as measured from the floor to the interior ceiling to provide for a generous space for retailing, services, and restaurant functions.
2. Facades of commercial and mixed-use buildings that face the street should be designed to be pedestrian friendly through the inclusion of at least three of the following elements:
 - Kick plates for storefront windows
 - Projecting window sills
 - Pedestrian-scale signage
 - Exterior lighting sconces
 - Containers for seasonal plantings
 - Window box planters
 - Benches and seat walls along 30% of the length of the façade
 - Decorative paving in the sidewalk
 - Decorative brick, tile or stone work on the ground floor façade
 - A feature not on the list that meets the intent of the guideline



2. Ground Level Transparency

Intent: To promote the use of building facades to provide safe and comfortable waiting areas for transit and provide visual connections between activities inside and out.

1. All commercial buildings should include windows with clear vision glass on at least 50 percent of the area between two and twelve feet above grade for all ground floor building facades that are visible from an adjacent street.
2. Street-facing, ground-floor facades of commercial and mixed-use buildings should incorporate generous amounts of glass in storefront-like windows. Amounts of clear, transparent glass should meet or exceed the following:
 - 80% along Type I streets
 - 60% along Type II
 - 50% along other streets



Merchandise and activity inside is highly visible from along the sidewalk.



3. Prominent Entrances

Intent: To design and orient building entrances that readily inform people of their access and use.

1. The primary (front) building façade and main entry of nonresidential buildings should be well-marked, articulated and oriented and facing the primary public street.
2. Consider placing the main building entrance at a street corner.
3. Entries should be lighted and protected from weather.
4. Entries facing public streets should be made visually prominent and receive architectural emphasis. A variety of techniques to accomplish this include:
 - recessed entries
 - projecting entries
 - elevated entries with stairways for residential uses
 - entry-related cover and/or roof line articulation (such as canopy articulation; parapet-roof articulation)
 - arched entries
 - decorative lintels of molding above doorways
 - landscape treatment and emphasis
 - surface treatment (such as paver or tiles)
 - entry courtyard
 - transom windows
 - signage
 - pilasters or columns supporting and/or framing the entrance
 - other techniques as appropriate



Trellis and landscape create a gracious main entry.



4. Treatment of Blank Walls

Intent: To ensure that buildings do not display blank, unattractive walls to the abutting street or public areas.

1. Use vegetation, such as trees, shrubs, ground cover and/or vines adjacent to the wall surface. Green walls are strongly encouraged to manage stormwater runoff.
2. The use of façade articulation such as expressing the structural bays of the building with pilasters or other detailing should be used to help animate an otherwise blank area of wall.
3. Use artwork, such as bas-relief sculpture, murals or trellis structures. Use seating areas with special paving.
4. Use architectural detailing, reveals, and contrasting materials.



Artwork can provide visual interest and reveal a place's culture and history.



Ground cover and vines help soften a flat facade.

5. Articulation

Intent:: To reduce the apparent bulk and maintain a human scale proportion in multi-story or large buildings.

1. Buildings should incorporate varied articulation on all sides. The street-facing side(s) should receive the greatest amount of attention with respect to richness of forms, details, materials, and craft.
2. Elements such as sun shades, terraces, and rain water harvesting features can be used to compose and articulate the building's façade.
3. Varied frontages. Building frontages should be divided into relatively small units with storefronts, bays, recesses, offsets, balconies, a varied and rich color palette, and other elements to avoid long, monolithic facades.



Frontage varied into bays and recesses.

6. Transition of Scale

Intent:

To encourage additional features to be incorporated into higher density development when located adjacent to properties with lower density single-family use to enhance the compatibility between uses.

1. Multi-family and mixed use development located adjacent to existing single-family residential should incorporate three or more of the following architectural features:
 - recessed entry
 - dormers
 - higher quality material
 - pitched roof forms
 - upper level balconies
 - upper level step backs
 - gables
 - window patterns
3. Flat, blank walls should not be visible from the street or common areas.
4. Tree retention or additional vegetative screening along neighboring properties is encouraged.



Multi-family buildings with architectural details matching the character and scale of the existing single-family structure.



Examples of multi-family development that is compatible with single family residential.

E. DESIGN GUIDELINES: SIGN DESIGN

1. Integration with Architecture

Intent: To ensure that signage is a part of the overall design approach to a project and not added as an afterthought element.

1. The design of buildings and sites shall identify location and sizes for future signs. As tenants install signs, it is expected that such signs shall be in conformance with an overall sign program that allows for advertising which fits the architectural character, proportions, and details of the development.



Sign integrated into the facade of the building.

2. Creativity / Unique Expressions

Intent: To encourage interesting, creative and unique approaches to the design of signs.

1. The design of signs are encouraged to use color, graphics, and handcrafted elements.

3. Pedestrian Orientation

Intent: To provide signs that will complement and strengthen the pedestrian realm.

1. Pedestrian signs include projecting signs (blade signs), window signs (painted on glass or hung behind glass), logo signs (symbols, shapes), wall signs over entrance, and monument signs.



Various artistically craft signs gives a pedestrian street added interest and character.

4. Coordinated Wayfinding

Intent: To ensure that public signage reflects and enhances the character of the district.

1. The City should implement a coordinated neighborhood identity program in the design of wayfinding signage.

